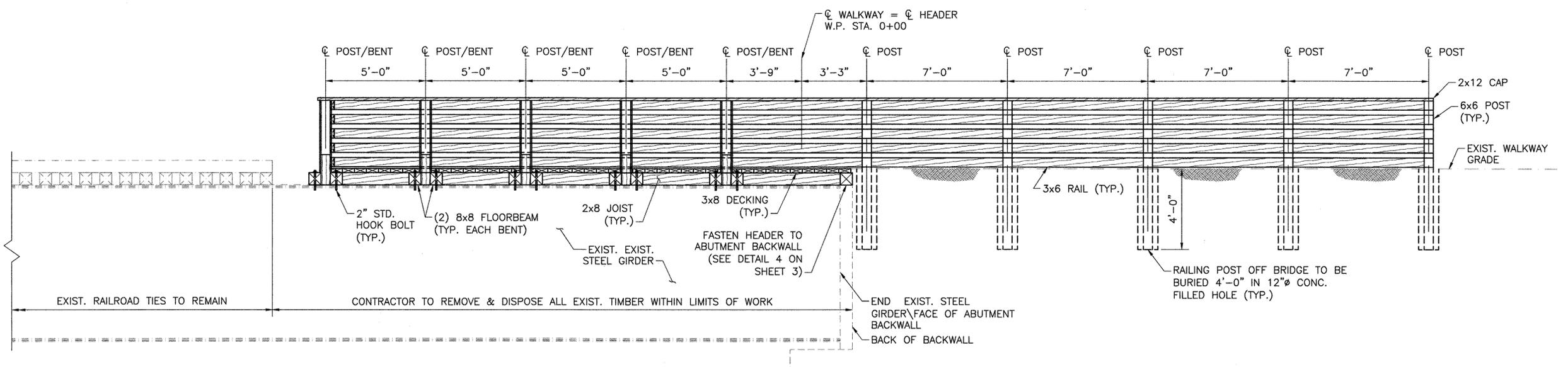


NOTES:
 1. PROVIDE 2x8 BLOCKING IN GAP BETWEEN 8x8'S AT EACH BENT LOCATION. BLOCKING SHALL BE TOE-NAILED IN PLACE AND KEPT IN SAME LINE AS 2x8 JOISTS.

OBSERVATION DECK PLAN
 SCALE: 3/8"=1'-0"

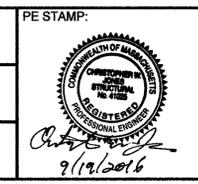


[LOOKING NORTH]
OBSERVATION DECK ELEVATION
 SCALE: 3/8"=1'-0"

C:\63003\5369 - NEWTON - GREENWAY OBSERVATION DECK\DRAWING FILES\SHEET1_NOTES\PLAN/ELEV/SECT.DWG

NUMBER	DATE	MADE BY	CHECKED BY	REVISIONS	DESCRIPTION

DRAWN BY:
TMW
 DESIGNED BY:
TMW
 CHECKED BY:
CJ



PREPARED BY:
 SCALE:
 AS SHOWN

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City of Newton, Massachusetts
Greenway Walking Corridor Observation Deck
OBSERVATION DECK PLAN AND ELEVATION

BETA JOB No. 5369
 PLOT DATE: 9/19/2016 11:56 AM
 ISSUE DATE: September 19, 2016
 SHEET No. 1
 File: Sheet1_NotesPlanElevSect.dwg

DESIGN DATA:

- 2009 AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO) LRFD GUIDE SPECIFICATION FOR THE DESIGN OF PEDESTRIAN BRIDGES, INCLUDING THE 2015 INTERIM REVISIONS
- 2009 INTERNATIONAL BUILDING CODE (IBC)
- MASSACHUSETTS AMENDMENTS TO THE 2009 INTERNATIONAL BUILDING CODE (8TH EDITION)
- 2012 NATIONAL DESIGN SPECIFICATIONS (NDS) FOR WOOD CONSTRUCTION

DESIGN LOADING:

90 PSF PEDESTRIAN LIVE LOAD.

RAIL AND POST LOADING CONSISTS OF 50 PLF OR 200 POUNDS, WHICHEVER GOVERNS, DISTRIBUTED IN ACCORDANCE W/ 2009 IBC 1607.7.1.

GENERAL NOTES:

1. ALL CONSTRUCTION INDICATED ON THESE PLANS SHALL BE IN ACCORDANCE WITH THE 1988 MASSDOT STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES W/ THE JULY 1, 2015 SUPPLEMENTAL SPECIFICATIONS.
2. DIMENSIONS AND ELEVATIONS ARE SHOWN TO THE NEAREST ONE-HUNDREDTH OF A FOOT OR ONE-EIGHTH OF AN INCH.
3. ANGLES ARE SHOWN TO THE NEAREST SECOND.
4. ALL WORKING POINTS ARE SHOWN AT THE CENTERLINE OF PATH UNLESS OTHERWISE NOTED.
5. NO INFORMATION WAS GIVEN FOR EXISTING UTILITIES. NO EXISTING UTILITIES ARE SHOWN ON THE PLANS. THE CONTRACTOR IS TO ASSUME THAT UTILITIES MAY BE PRESENT.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COORDINATION WITH ALL UTILITY COMPANIES.
7. ANY DAMAGE TO EXISTING CITY OR PRIVATE PROPERTY CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CITY.
8. THE CONTRACTOR SHALL PLACE ALL EQUIPMENT AND MATERIAL IN HIS FIELD YARD OR AT A SITE APPROVED BY THE CITY. THE EQUIPMENT AND MATERIAL SHALL BE PLACED IN A STORAGE AREA SO AS NOT TO CAUSE A SAFETY HAZARD.
9. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY ALL ELEVATIONS, DIMENSIONS, DETAILS, ANGLES, STRUCTURAL MEMBER SIZES, AND LAYOUTS AS SHOWN ON THESE PLANS. THIS PRIOR FIELD VERIFICATION IS ESPECIALLY PERTINENT FOR PRE-FABRICATED STRUCTURAL ITEMS AND WORK IN THE VICINITY OF UTILITIES (IF APPLICABLE).
10. FEDERAL LAW REQUIRES NOTIFICATION OF APPROPRIATE UTILITY COMPANIES BEFORE DIGGING, TRENCHING, BLASTING, DEMOLISHING, BORING, BACKFILLING, GRADING, LANDSCAPING, OR OTHER EARTH MOVING OPERATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES (INCLUDING THROUGH THE "DIG SAFE" PROGRAM) TO ENSURE THAT ALL UTILITIES, BOTH UNDERGROUND AND OVERHEAD, HAVE BEEN MARKED BEFORE COMMENCEMENT OF SUCH WORK. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE "DIG SAFE" PROGRAM. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANIES, SHALL BE REPAIRED OR REPLACED (AS DEEMED APPROPRIATE BY THE IMPACTED UTILITY COMPANY) AT NO ADDITIONAL COST TO THE CITY.
11. FOR STRUCTURAL DETAILING, THE FOLLOWING MEMBER DIMENSIONS WERE ASSUMED:

MEMBER	ACTUAL DIMENSIONS
1x6 KICKER	3/4" x 5 1/2"
2x12 TOP RAIL	1 1/2" x 11 1/4"
2x8 JOIST	1 1/2" x 7 1/4"
3x8 DECKING	2 1/4" x 7 1/4"
3x6 RAIL	2 1/4" x 5 1/2"
6x6 POST	5 1/2" x 5 1/2"
6x8 BLOCKING	5 1/2" x 7 1/4"
8x8 FLOORBEAM	7 1/4" x 7 1/4"

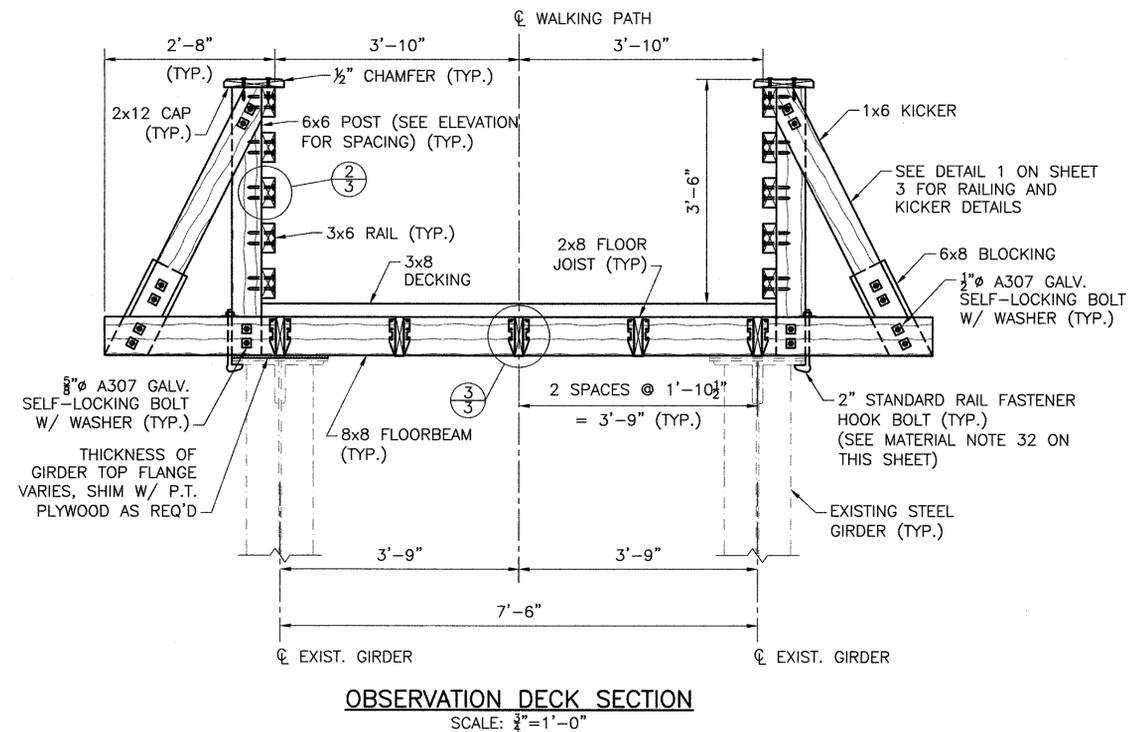
IF ACTUAL MEMBER DIMENSIONS VARY, ADJUST ACCORDINGLY.

TEMPORARY PROTECTIVE SHIELDING NOTES:

12. TEMPORARY PROTECTIVE SHIELDING SHALL BE PROVIDED DURING DEMOLITION OF THE EXISTING STRUCTURE AND DURING THE CONSTRUCTION OF THE NEW STRUCTURE. SHIELDING SHALL BE INSTALLED PRIOR TO COMMENCING ANY WORK.
13. THE CONTRACTOR IS RESPONSIBLE FOR PREVENTING ANY DEBRIS RESULTING FROM DEMOLITION, EXCAVATION, OR CONSTRUCTION FROM FALLING INTO THE RIVER BELOW. ANY DEBRIS THAT FALLS TO THE RIVER BELOW DUE TO THE CONTRACTORS ACTIVITIES SHALL BE IMMEDIATELY REMOVED BY THE CONTRACTOR AT HIS/HER EXPENSE.
14. THE TEMPORARY SHIELDING SHALL BE PLACED IN A MANNER AS TO PREVENT IT FROM BEING BLOWN OUT BY WIND. IF, IN THE OPINION OF THE CITY, THE SHIELDING IS NOT SECURE, THE CONTRACTOR SHALL REMOVE IT AND INSTALL IT TO THE SATISFACTION OF THE CITY AT NO ADDITIONAL COST.
15. SHIELDING SHALL BE DESIGNED TO SAFELY WITHSTAND ALL LOADS THAT IT MAY BE SUBJECTED TO; AND BE SEALED TIGHTLY AT ALL JOINTS.
16. ALL MATERIALS USED IN THE SHIELDING SYSTEM SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED FROM THE SITE AT THE COMPLETION OF THE PROJECT.
17. THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OF ALL PROPOSED SHIELDING TO THE CITY OF NEWTON CONSERVATION COMMISSION FOR APPROVAL PRIOR TO INSTALLATION.

MATERIAL NOTES:

18. ALL TIMBER SHALL BE SAWN LUMBER, SURFACED FOUR SIDES (242) UNLESS OTHERWISE NOTED, AND SHALL COMPLY WITH THE REQUIREMENTS OF AASHTO M168.
19. ALL TIMBER SHALL BE PRESSURE TREATED WITH ACQ CONFORMING TO AWPA STANDARD P5. ALL MEMBERS SHALL BE FABRICATED BEFORE TREATMENT AND DRIED TO A MOISTURE CONTENT OF 19 PERCENT OR LESS AFTER TREATMENT.
20. ALL TIMBER SHALL BE PREDRILLED PRIOR TO TREATMENT.
21. FLOORBEAMS, JOISTS, DECKING, BLOCKING, KICKERS, POSTS, AND RAILS SHALL BE SOUTHERN PINE, GRADE NO. 1 OR GREATER.
22. ALL TIMBER SHALL BE PRECISION END TRIMMED TO LENGTH WITH 1/4" UNDER LENGTH AND NO OVER LENGTH TOLERANCE PERMITTED.
23. ALL TREATED TIMBER THAT IS FIELD CUT, BORED THRU, DRILLED INTO OR DAMAGED SHALL BE TREATED AS OUTLINED IN AWPA STANDARD M4 WHICH REQUIRES THAT ALL CUTS, HOLES, OR INJURIES TO TREATED WOOD BE PROTECTED BY BRUSHING, SPRAYING, DIPPING, OR SOAKING IN AN APPROVED PRESERVATIVE.
24. ALL BOLTS SHALL CONFORM TO ASTM A307, UNLESS OTHERWISE NOTED. LAG SCREW SHALL COMPLY WITH THE REQUIREMENTS OF ANSI/ASME STANDARD B18.2.1, GRADE 2.
25. ALL HARDWARE SHALL BE GALVANIZED AS PER CURRENT STATE SPECIFICATIONS AND/OR AASHTO SPECIFICATION M232.
26. CONSTRUCTION REQUIREMENTS SHALL CONFORM TO STATE SPECIFICATIONS. ALL TIMBER SHALL BE CUT TO LENGTH AND DRESSED TO SIZE REQUIRED PRIOR TO TREATMENT.
27. TIMBER WHICH AT THE DISCRETION OF THE CITY IS SEVERELY WARPED, BOWED, SPLIT, OR SPLINTERED SHALL NOT BE INCORPORATED IN THE WORK.
28. TIMBER RAIL POST SHALL BE SET VERTICAL IN THE FIELD.
29. TIMBER RAILS SHALL BE ATTACHED PARALLEL TO THE ACTUAL SLOPE OF DECK.
30. RAILS SHALL BE CONTINUOUS OVER TWO POSTS SPACING (MINIMUM).
31. STEEL PLATES SHALL CONFORM TO ASTM A606.
32. 2" RAIL FASTENING HOOK BOLTS SHALL BE 3/8"Ø AND GALVANIZED BY LEWIS BOLT AND NUT COMPANY OR YANGTZE RAILROAD MATERIALS. ALTERNATE MANUFACTURERS MAY BE PERMITTED UPON APPROVAL FROM THE CITY.
33. EXPANSION ANCHORS SHALL BE HOT-DIPPED GALVANIZED IN ACCORDANCE WITH ASTM A 153. NUTS AND WASHERS SHALL BE IN ACCORDANCE WITH ASTM A 563, GRADE A, HEX AND ASTM F 844, RESPECTIVELY.



OBSERVATION DECK SECTION
SCALE: 3/4"=1'-0"

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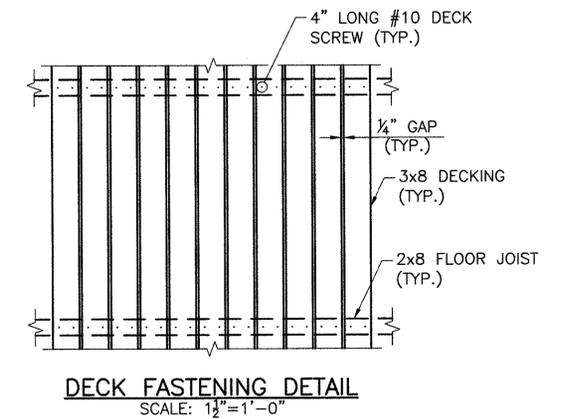
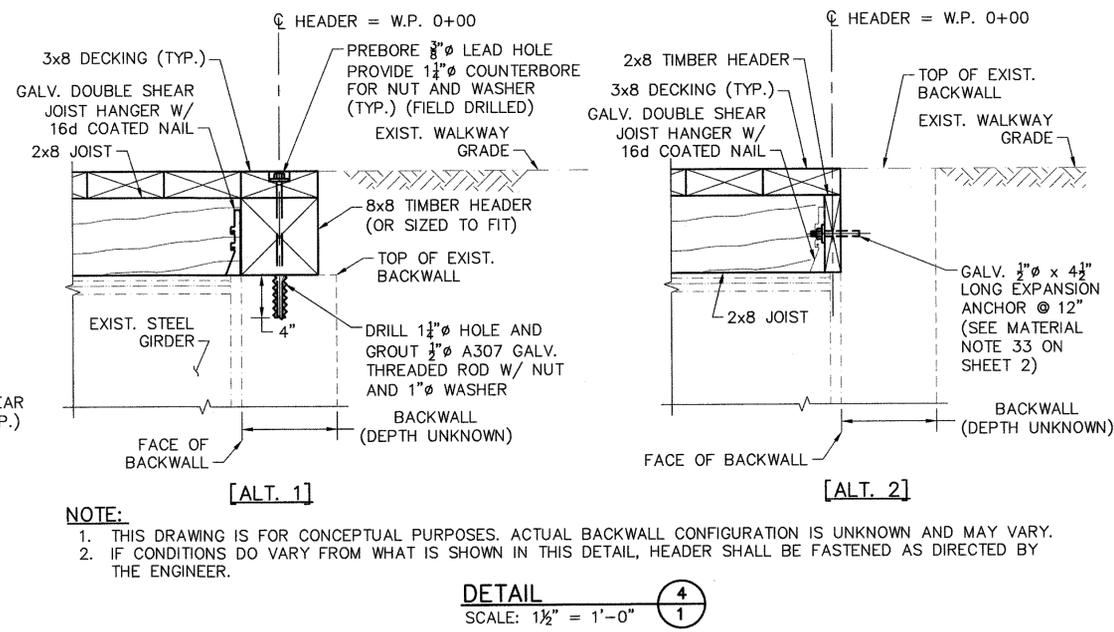
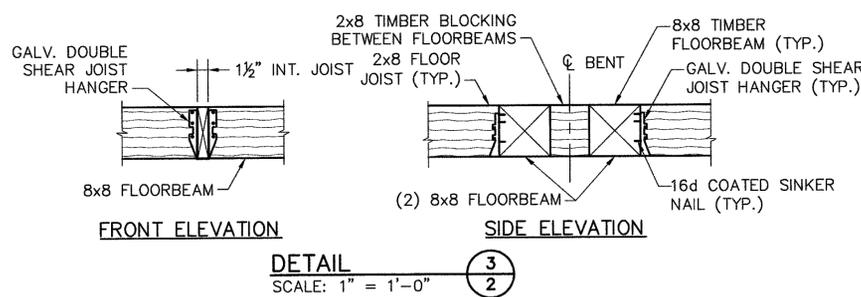
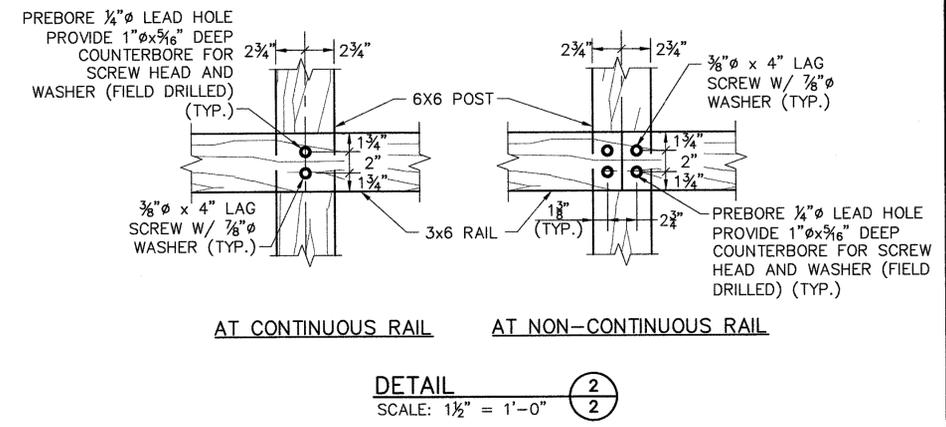
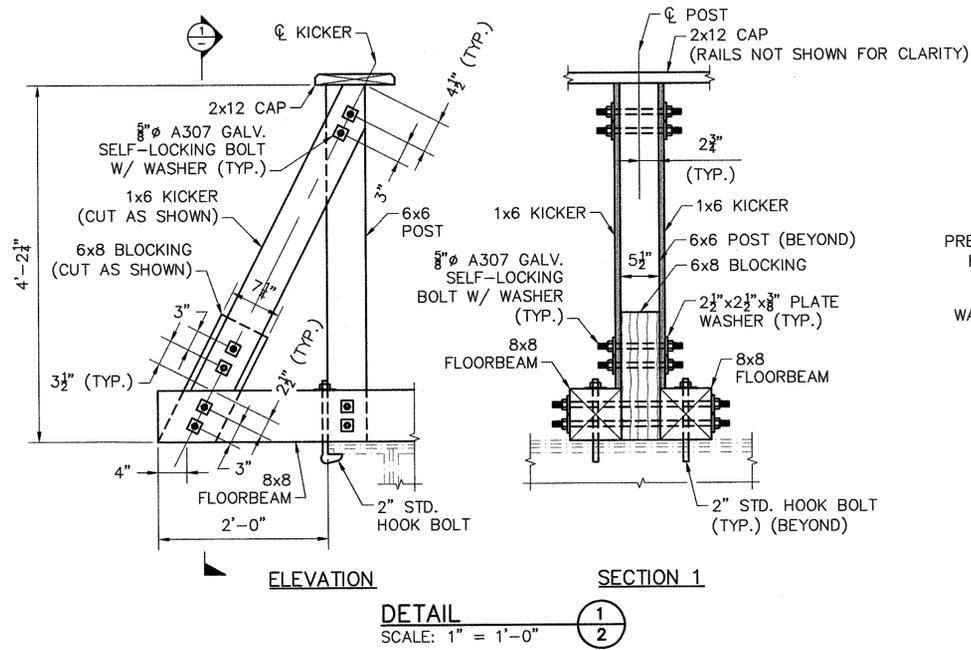
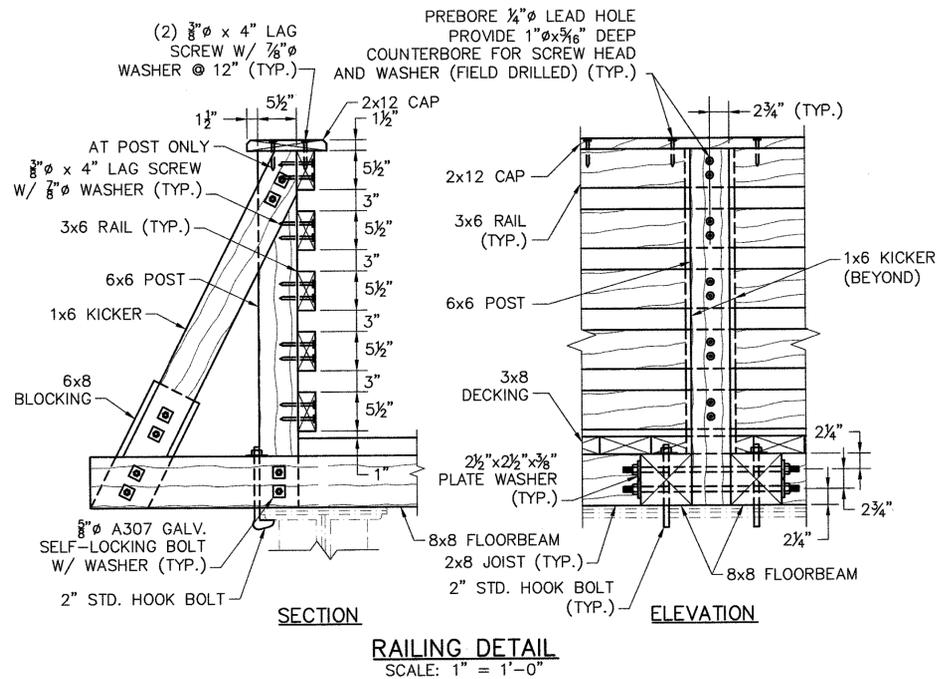
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Greenway Walking Corridor Observation Deck
NOTES AND OBSERVATION DECK SECTION

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DESIGNED BY: TMW
CHECKED BY: CJ

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City of Newton, Massachusetts
Greenway Walking Corridor Observation Deck
OBSERVATION DECK DETAILS

BETA JOB No.	5369
PLOT DATE:	9/19/2016 11:56 AM
ISSUE DATE:	September 19, 2016
SHEET No.	3
File:	Sheet3_Details.dwg